

# Chapter 5

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## CATALYTIC PROJECTS & INITIATIVES

### INTRODUCTION

Throughout the planning and public input process for the West Side Area Plan, several redevelopment projects and programs aimed at increasing both economic value and community identity were identified. The implementation of these “catalytic” projects and programs will represent a significant financial investment in the neighborhood, enhance the visual character of the West Side, and potentially attract additional reinvestment in neighboring properties.

A series of catalytic projects have been selected for the West Side, as illustrated in Figure 5-1, including: redevelopment principles and concepts for commercial corridors and gateways; redevelopment opportunities and illustrated design principles for local business and neighborhood shopping districts; a neighborhood improvement demonstration project; and summaries of two significant ongoing planning/implementation projects.

Each recommended project and initiative provides users with a community-supported vision, as well as general guidelines and tools for redevelopment. In some cases, illustrated design concepts

have been included to provide a sample vision for the selected site and to supplement the text descriptions. The spirit of these concepts is to assist property owners, developers, investors, and City departments in understanding and supporting a consensus-based vision for the area. The illustrations explain and provide detailed examples that are consistent with the goals, objectives, and policies identified in previous Plan chapters. However, it is important to understand that the catalytic design concepts are not intended to constitute finalized site plans or redevelopment projects. They do not negate individual property rights or zoning requirements, nor do they represent an intent to override ongoing planning efforts. The concepts are a useful tool for depicting design alternatives at different levels of investment or for different approaches to development or “build-out.” They typically include guiding principles for long-term economic success and environmental sustainability of the area.

In summary, catalytic projects for the West Side were identified through the public input process and selected for (1) their ability to have a large, significant and positive economic impact on the immediate and surrounding area, e.g., job creation,

improvements to services, shopping, or quality-of-life for Milwaukee residents; (2) their potential for leveraging investment both in the project area and in surrounding or spin-off projects that add long term value to the property tax base; and (3) their demonstrated basis for partnership with both the private sector businesses and community-based organizations in the project area, to ensure that project goals are met and a high standard of development is achieved.

- Mosaic on Burleigh - Visioning & Strategic Planning for the Burleigh Main Street District and BID #27 (*page 145*)

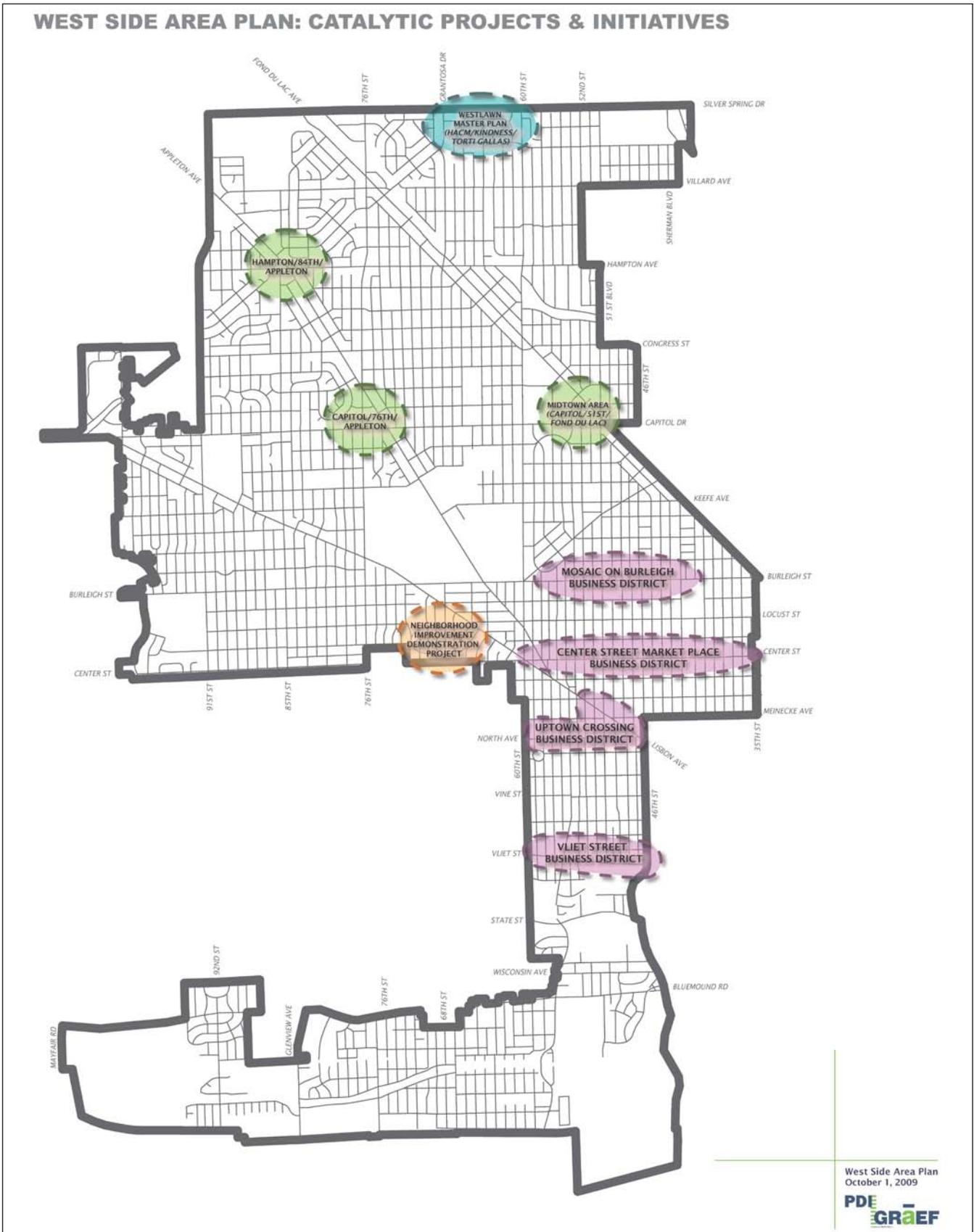
## **CATALYTIC PROJECT STRUCTURE**

Each catalytic project or initiative includes: a detailed description of existing conditions; an overall vision for the selected study area; a series of strategies and recommendations, which may include an illustrated design concept; identified responsible parties; and a recommended timeline for implementation. The aforementioned design concepts are intended to be illustrative possibilities or design scenarios for how the project goals could be achieved.

### ***Catalytic Projects and Initiatives Include:***

- Gateway Zones & Aging Commercial Corridors (*page 108*)
  - Midtown Gateway Area (*page 109*)
  - Capitol Drive / 76th Street / Appleton Avenue (*page 113*)
  - Hampton Avenue / Appleton Avenue / 84th Street / Grantosa Drive (*page 118*)
- Local Business & Neighborhood Shopping Districts (*page 123*)
  - Redevelopment Opportunities (*page 123*)
  - Illustrated Design Manual (*page 142*)
- Neighborhood Improvement Demonstration Project (*page 143*)
- Major & Ongoing Initiatives (*page 144*)
  - Master Plan for the Revitalization of Westlawn (Housing Authority of the City of Milwaukee) (*page 144*)

Figure 5-1. Catalytic Projects & Initiatives



## **GATEWAY ZONES & AGING COMMERCIAL CORRIDORS**

The West Side plan area includes several commercial corridors, many of which converge or intersect to create significant nodes of activity, shopping districts and gateways to the various neighborhoods. In these circumstances, “Gateway Zones” are formed and present special opportunities - and, at times, challenges - to the businesses, property owners, and various users of the corridors. The first set of catalytic projects identifies the components of gateway zones and then applies the concept to three specific areas within the Plan boundary. In addition to diagramming the gateway zones for the three target study areas, illustrated design concepts are included to show how the concept could be applied within the built environment.

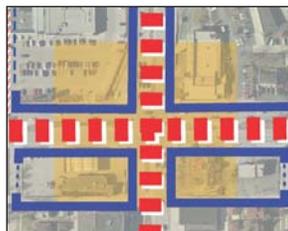
### ***Introduction: Gateway Zones***

Gateway Zones offer opportunities for creating highly visible improvements and establishing community identity at key intersections. Within each “gateway,” there may be several zones - each with its own characteristics and purpose. The intent of identifying Gateway Zones is to identify possible redevelopment and/or property enhancement opportunities, set a framework to support the long term vitality and sustainability of the corridors/nodes over the long-term, and to create a sense of place for the neighborhoods.

The following zones may be found within a “Gateway Zone”:

### ***Intersection Zones***

The Intersection Zone (highlighted in yellow) is the most visible part of the gateway with the most potential for establishing an inviting character and distinct identity.



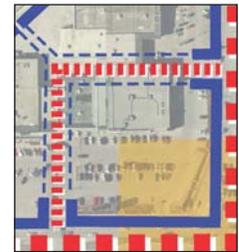
### ***Block Zones***

Block Zones (illustrated with heavy, solid blue lines) are contiguous pieces of property where there are opportunities for joint improvement efforts between businesses and property owners.



### ***Block Transition Zones***

Block Transition Zones (illustrated with dashed blue lines) include properties where there may be opportunities for “multi-block” investment as a result of property consolidation or joint improvement plans by adjacent property owners and businesses.



### ***Arterial Traffic Corridors***

Primary business corridors (indicated by heavy red dashed lines) with significant, multi-modal traffic. Investments along arterial traffic corridors should be coordinated between property owners and businesses in order to create an impression of continuous investment and identity.



### ***Secondary Traffic Corridors***

Secondary Traffic Corridors (indicated by smaller red dashed lines) link to major arterials and may offer opportunities for smaller, lower-intensity investment, such as improvements to existing properties and/or redevelopment at the discretion of property owners.



## **GATEWAY ZONES & AGING COMMERCIAL CORRIDORS**

### **MIDTOWN GATEWAY AREA**

#### ***Existing Conditions***

The Midtown Area is generally located within the boundary of Capitol Drive, 51st Street, Fond du Lac Avenue, and 60th Street. Formerly home to the Capitol Court Shopping Center, this area went through an extensive transformation and redevelopment in 2001, allowing confidence to be restored to the area and its surrounding neighborhoods.



High traffic counts and strong residential density create a vibrant market for the Midtown Area, along with recent commercial and retail investments. The intersection of Capitol, 51st, and Fond du Lac makes this visibly active area a prime location for



additional investment and redevelopment due to its readily accessible location for both vehicular and pedestrian traffic.

Building on the recent improvements of this area will: strengthen its identity; support its long-term vitality; and encourage property investment and redevelopment. Ultimately, this benefits not only the Midtown Area, but increases and stabilizes the value of the neighborhood at large.



#### ***Vision***

The Midtown Area includes several strong assets; this catalytic project highlights opportunities to capitalize upon these strengths and extend them to the rest of the Midtown Triangle and along the adjacent corridors. Critical emphasis is given to the Town Square/Main Street design of the Midtown Center which could be extended or expanded upon; gateways (see *Intersection Zones*) which heighten awareness of the district and strengthen the area's identity; and the potential of the surrounding commercial corridors for increasing the Midtown area's attractiveness as a retail destination.

The goals of this catalytic project include (but are not limited to):

- Expand upon Midtown Center's success
- Heighten visual prominence and establish gateway identities
- Identify opportunities for redevelopment and property enhancement
- Improve pedestrian amenities and continue to support transit
- To the extent possible, extend the Midtown design concept (walkable, pedestrian-friendly, Main Street) to the surrounding area
- Increase land value and provide stability to the area
- Improve the perception of the area surrounding Midtown Center

## ***Midtown Gateway Area***

The following gateway zones are described below and illustrated for the Midtown Gateway Area in Figure 5-2.

### **Intersection Zones**

The intersection zone of Capitol and Fond du Lac should include a strong gateway feature that establishes a distinct identity for the Midtown area and draws upon the character of the Midtown shopping center. This could include significant streetscaping and/or signage. As an alternative to using a landscape feature (at the property owner's discretion) - a signature building could become a gateway feature for the Midtown triangle. For example, an architecturally significant building with a zero setback could serve as a visual landmark for the area. A signature bus shelter could at a minimum, add an identifying feature.



*Existing median at the intersection of Capitol Dr., Fond du Lac Ave, and 51st St. Applying recommendations for the intersection zone, the median could be improved with improved landscaping to screen utilities and define the pedestrian zone. Alternatively, if the above ground utility boxes are not required, a public art feature could become a welcoming gateway feature (see image at right from the Uptown Crossing BID).*



### **Block Zones**

Within the block zones, a contiguous streetscape program should be encouraged along the pedestrian right-of-way. Surface parking between the principal building facade and the pedestrian right-of-way should be discouraged along the block zone, located behind the buildings or concentrated in the middle of the zone, so as to minimize interruption to the building pattern.

### **Block Transition Zones**

Block transition zones highlight opportunities for properties to establish a joint improvement or redevelopment proposal. In these cases, the improvement would have an impact on multiple block faces at once and represents a much larger coordinated effort. There are also opportunities to incorporate public plazas and/or open spaces into these larger redevelopment projects.

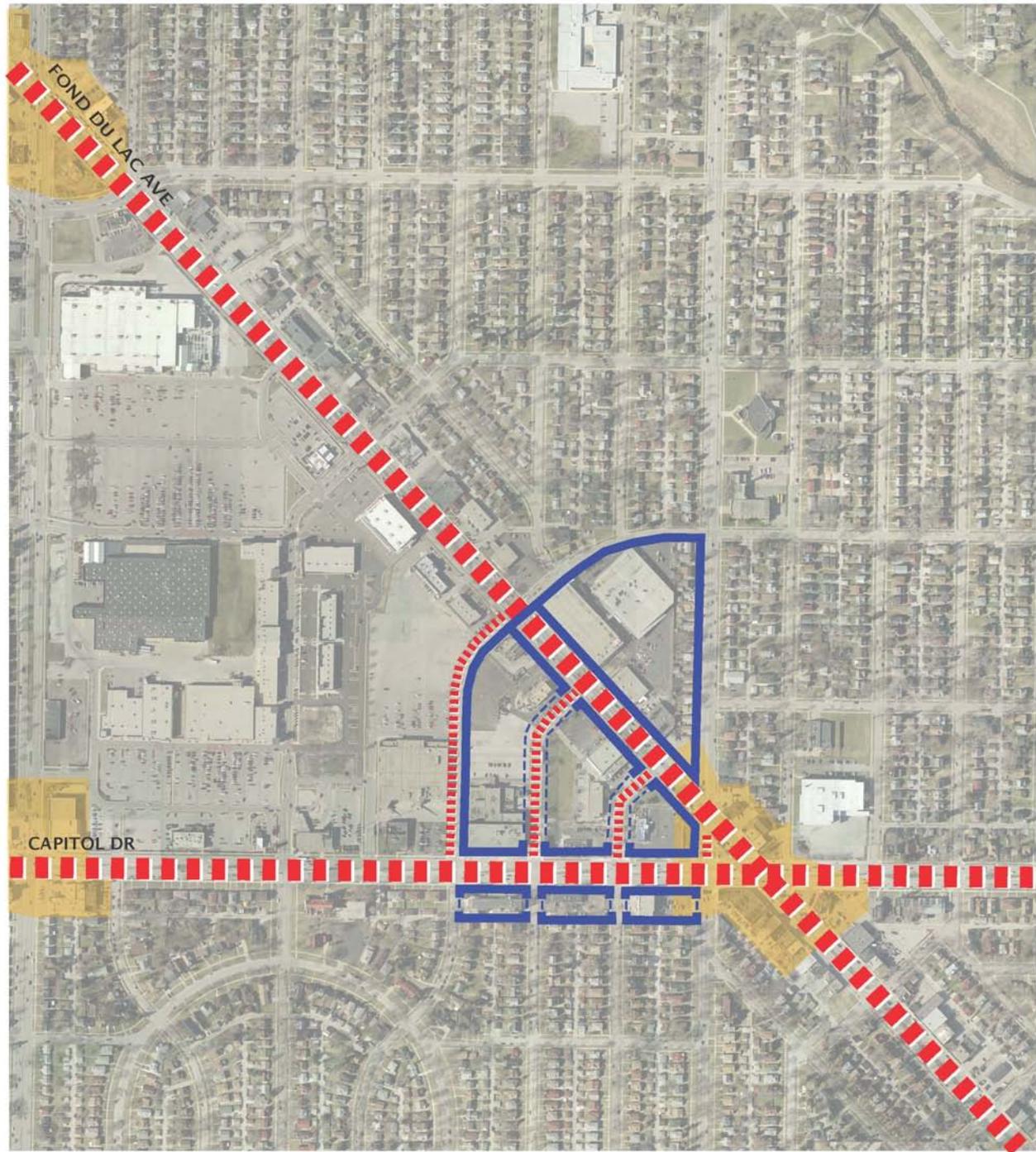
### **Arterial Traffic Corridors**

Compatible architectural styles should be utilized along the arterial traffic corridor to create an identity for the Midtown area. Increased streetscape and landscaping should also be introduced, as well as key points-of-entry for each building.

### **Secondary Traffic Corridors**

The secondary traffic corridors should incorporate access to surface parking, as well as secondary points-of-entry for each building. If service areas are necessary, they should be accessed from the secondary traffic corridor and adequately screened with landscaping or high-quality fencing materials.

Figure 5-2. Midtown Gateway Area



**GATEWAY ZONES: MIDTOWN AREA (CAPITOL DR, 51ST ST, FOND DU LAC AVE, 60TH ST)**

**Legend**

-  Intersection Zones
-  Block Zones
-  Block Transition Zones
-  Arterial Traffic Corridors
-  Secondary Traffic Corridors

## Strategies & Recommendations

The catalytic diagram created for the Midtown Area (Figure 5-3) focuses around the intersection of Capitol Dr. with Fond du Lac Ave and 51st St. and illustrates the following recommendations:

- Establish a significant pedestrian zone at the primary intersection, which could include landscaping, pedestrian amenities, and traffic calming measures to increase pedestrian safety
- Encourage additional streetscaping, which may include planters within the sidewalk or garden-style boulevards within the medians, along the primary corridors
- When there are opportunities for infill redevelopment or business expansion, encourage buildings to reinforce the street edge and locate parking to the side or rear of the principal building facade.
- Support and expand facade grant opportunities for the properties within the Midtown area, which further supports the identity of the larger area

- Incorporate pedestrian and bicycle linkages between the neighborhoods and shopping/service areas, when possible
- Improve vehicular circulation by limiting curb cuts and consolidating access drives as much as possible

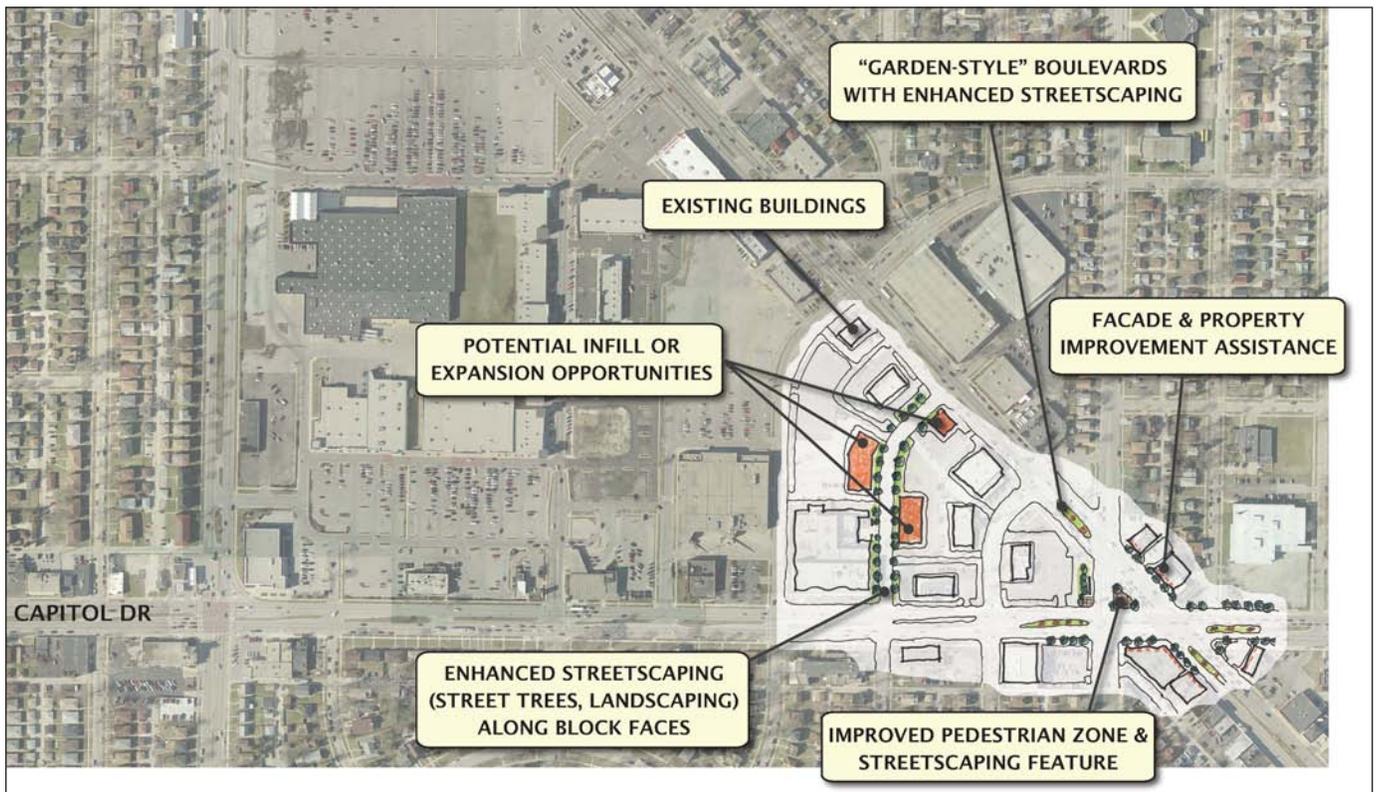
## Responsible Parties

- Developer(s)
- Property Owner(s)
- Department of City Development
- Redevelopment Authority of the City of Milwaukee
- Department of Public Works

## Timing

This option should begin with a discussion between the City, Midtown Business Association, and other key stakeholders to analyze and determine potential opportunities that are a result of the current economic climate and local market conditions.

Figure 5-3. Midtown Gateway Area Concept



**GATEWAY ZONES & AGING COMMERCIAL CORRIDORS**  
**CAPITOL DRIVE / 76TH STREET / APPLETON AVENUE**

**Existing Conditions**

The intersection of Capitol Dr., 76th St., and Appleton Ave represents a major node with high volumes of both local and commuter vehicular traffic. This active area is a prime location for development due to its exceptional visibility and accessibility, as well as its proximity to several established neighborhoods. In addition to retail



and office uses, there are many options for transit and civic services available within the study area. Strengthening the identity of this node, while encouraging a stable mix of uses, will increase the economic and social value of the immediate area, as well as the surrounding neighborhoods.



**Vision**

The vision for this active destination node focuses on enhancing the positive aspects of the area by capitalizing on its strengths. Through this process, the node will develop a distinct identity as a destination - this is something the neighborhood residents currently feel is lacking along many of the auto-oriented corridors.

The goals of this catalytic project include (but are not limited to):

- Heighten visual prominence and establish gateway identities
- Increase land value and provide stability to the area
- Identify opportunities for redevelopment and property enhancement
- Encourage additional investment along the primary corridors (Capitol Dr., 76th St., and Appleton Ave)
- Provide high quality residential options where opportunities exist to blend residential with commercial uses.
- Create public green space/open space, plazas, and/or landscaped courtyards.
- Improve pedestrian amenities and continue to support transit
- Work with property owners to address building and landscape code violations. Bring all properties up to code.
- Improve the perception of the area

### ***Capitol Drive /76th Street /Appleton Avenue Gateway Area***

The following gateway zones are described below and illustrated for the Capitol Drive/76th Street/Appleton Avenue Gateway Area in Figure 5-4.

#### **Intersection Zones**

The intersection zones displayed on the diagrams should all include strong gateway features that establish a clear identity for the Capitol/76th/Appleton area. Opportunities could include a building, significant landscaping, signage, or other special feature located at the street edge in the specific locations noted in the diagram.

#### **Block Zones**

Within the block zones, a contiguous streetscape program of improvements should be encouraged along the pedestrian right-of-way. Surface parking between the principal building facade and the pedestrian right-of-way should be discouraged along the block zone, located behind the buildings or concentrated in the middle of the zone, so as to minimize interruption to the building pattern.

#### **Block Transition Zones**

Block transition zones highlight opportunities for properties to establish a joint improvement or redevelopment proposal. In these cases, the improvement would have an impact on multiple block faces at once and represents a much larger coordinated effort. There are also opportunities to incorporate public plazas and/or open spaces into these larger redevelopment projects.

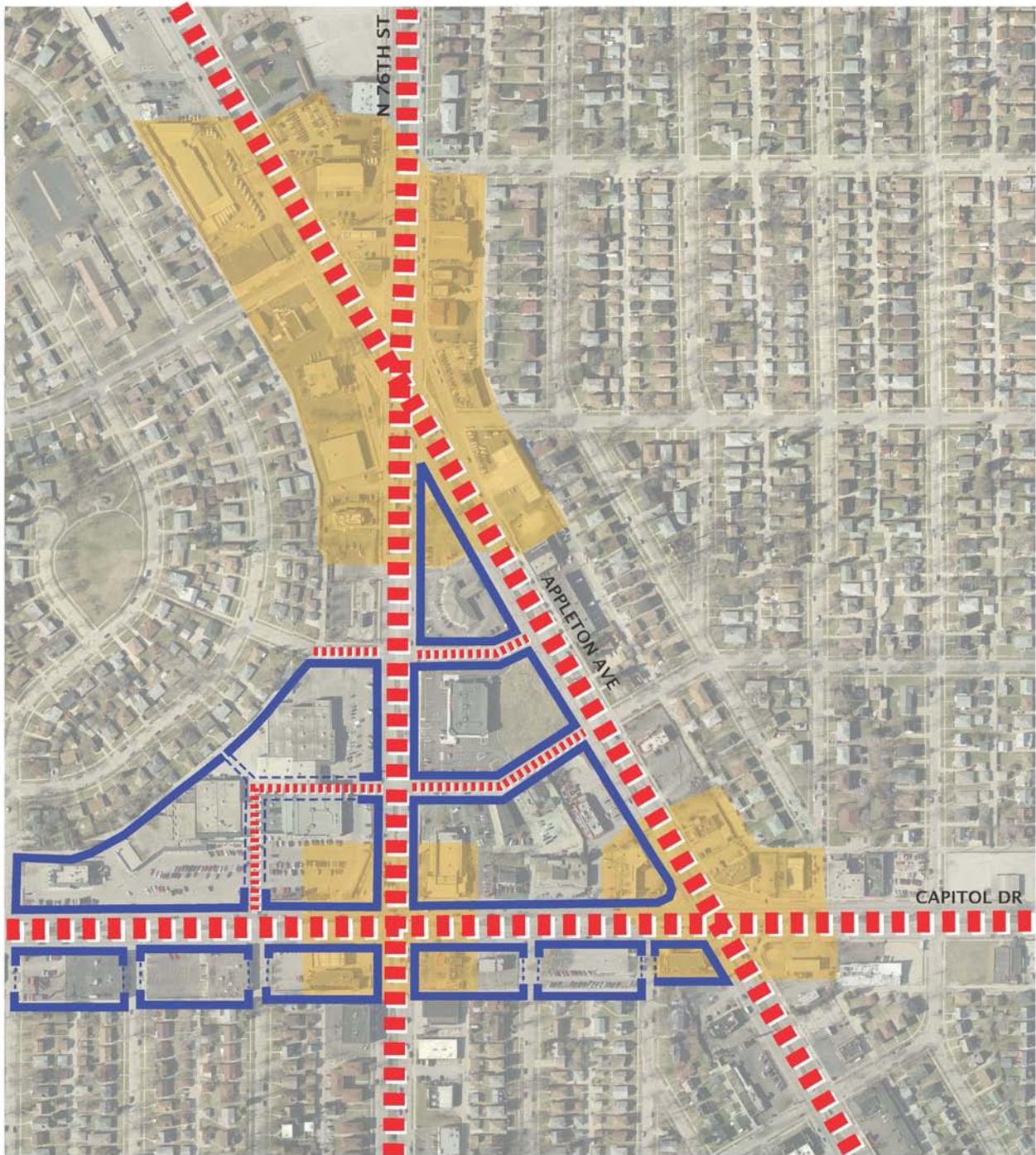
#### **Arterial Traffic Corridors**

Compatible architectural styles should be utilized along the arterial traffic corridor to create an identity for the Capitol Drive area. Increased streetscape and landscaping should also be introduced, as well as key points-of-entry for each building.

#### **Secondary Traffic Corridors**

The secondary traffic corridors should incorporate access to surface parking, as well as secondary points-of-entry for each building. If service areas are necessary, they should be accessed from the secondary traffic corridor and adequately screened with landscaping or high-quality fencing materials.

Figure 5-4. Capitol Drive/76th Street/Appleton Avenue Gateway Area



**GATEWAY ZONES: CAPITOL DRIVE - 76TH STREET - APPLETON AVENUE**

**Legend**

- Intersection Zones
- Block Zones
- Block Transition Zones
- Arterial Traffic Corridors
- Secondary Traffic Corridors

West Side Area Plan



### Strategies & Recommendations

Two alternate catalytic diagrams were created for the Capitol Drive / 76th Street / Appleton Avenue Gateway Area. The following bullet points describe the alternatives presented in Concept 1 (Figure 5-5) and Concept 2 (Figure 5-6).

#### Concept 1

- Establish a significant pedestrian zone at the primary intersections, which could include landscaping, pedestrian amenities, and traffic calming measures to increase pedestrian safety
- Introduce a gateway feature at the intersection of Appleton and Capitol
- Eliminate the right-turn bypass lane at the northwest corner of the intersection of Capitol and 76th Street
- Where there are opportunities for infill redevelopment or business expansion, encourage buildings that reinforce the street edge by locating parking behind or alongside the building
- Include a “green” focal point within the parking courtyard of the block that is defined by Capitol to the south and Appleton to the east, which also provides a green link into the adjacent neighborhoods
- Incorporate pedestrian and bicycle connections between the neighborhoods and shopping/service areas
- Improve vehicular circulation by limiting curb cuts and consolidating access drives

Figure 5-5. Capitol Drive/76th Street/Appleton Avenue Gateway Area Concept 1



### **Concept 2**

In addition to the strategies and recommendations shown in Concept 1, the alternative for Concept 2 proposes a different building configuration with new, high-quality residential options and integrated green space/open space, linking into adjacent neighborhoods.

### **Timing**

This option should begin with a discussion between the City, developers, investors, neighborhood groups, and area property owners to analyze and determine potential opportunities that are a result of the current economic climate and local market conditions.

### **Responsible Parties**

- Developer(s)
- Property Owner(s)
- Department of City Development
- Redevelopment Authority of the City of Milwaukee
- Department of Public Works

Figure 5-6. Capitol Drive/76th Street/Appleton Avenue Gateway Area Concept 2



**GATEWAY ZONES & AGING COMMERCIAL CORRIDORS**  
**HAMPTON AVENUE / APPLETON AVENUE /**  
**84TH STREET / GRANTOSA DRIVE**

**Existing Conditions**

The intersection of Hampton Ave, Appleton Ave, and Grantosa Dr. is well traveled with significant traffic counts, commercial uses, residential uses, and pedestrian activity. The intersection’s proximity to US 45, STH 145, and the Lawrence J. Timmerman Airport makes this active node a prime location for



development due to its exceptional visibility and accessibility. An existing vacant building (former Kohl’s store), as well as underutilized parking lots, provide the opportunity to re-envision the area as a destination for the adjacent neighborhoods as well as the larger community.



**Recent Developments**

Given Milwaukee County’s current budget and planned cuts for the Milwaukee Public Library System, there is a real likelihood that the Capitol and Mill Road Libraries will be closed at some point in the near future. Based on that eventuality, there is a compelling argument for taking the vacant Kohl’s store at the intersection of Appleton and Hampton and making it the site of a new larger library. The lease space has been unused for several years. It has excellent parking and access from two major streets. The structure is in good

condition and may make the site a good candidate for a building retrofit to create a library rather than build a new structure from the ground up. To add to the viability of this re-use, a developer could place a coffee shop with an expanded seating area in an adjacent space to add to the attractiveness of the library. This redevelopment strategy is explicitly mentioned under Concept 1 as a way to reinvest in the existing “medium box” that is the former Kohl’s store.

**Vision**

The vision for the Hampton Avenue / Appleton Avenue / 84th Street / Grantosa Drive node focuses on building upon existing strengths, reusing underutilized spaces, and integrating the neighborhood into the study area. The intent is to develop a unique identity for the area, which is something that many residents expressed is lacking.

The goals of this catalytic project include (but are not limited to):

- Establish an identifiable character for the area
- Increase land value and provide stability to the area
- Encourage new investment along Hampton and Appleton Avenues
- Identify opportunities for redevelopment and property enhancement, including the reuse of vacant or underutilized properties
- Provide opportunities for high quality residential options that will blend into the existing neighborhoods
- Use green spaces, community gathering places and a strong pedestrian network to link surrounding neighborhoods to the center of commercial activity or “destination node”
- Improve pedestrian amenities and overall walking environment
- Continue to support transit and transit-oriented development
- Improve the perception of the area

***Hampton Avenue /Appleton Avenue /84th Street /Grantosa Drive Gateway Area***

The following gateway zones are described below and illustrated for the Hampton Avenue/Appleton Avenue/84th Street/Grantosa Drive Gateway Area in Figure 5-7.

**Intersection Zones**

The intersection zones displayed on the diagrams should all include strong gateway features that establish a strong character for the Hampton/Appleton/Grantosa area. Opportunities could include a building, significant landscaping, signage, or other special feature located at the street edge in the specific locations noted in the diagram.

**Block Zones**

Within the block zones, a contiguous streetscape program of improvements should be encouraged along the pedestrian right-of-way. Surface parking between the principal building facade and the pedestrian right-of-way should be discouraged along the block zone, located behind the buildings or concentrated in the middle of the zone, so as to minimize interruption to the building pattern.

**Block Transition Zones**

Block transition zones highlight opportunities for properties to establish a joint improvement or redevelopment proposal. In these cases, the improvement would have an impact on multiple block faces at once and represents a much larger coordinated effort. There are also opportunities to incorporate public plazas and/or open spaces into these larger redevelopment projects.

**Arterial Traffic Corridors**

Compatible architectural styles should be utilized along the arterial traffic corridor to create an identity for the area. Increased streetscape and landscaping should also be introduced, as well as key points-of-entry for each building.

**Secondary Traffic Corridors**

The secondary traffic corridors should incorporate access to surface parking, as well as secondary points-of-entry for each building. If service areas are necessary, they should be accessed from the secondary traffic corridor and adequately screened with landscaping or high-quality fencing materials.

Figure 5-7. Hampton Avenue/Appleton Avenue/84th Street/Grantosa Drive Gateway Area



## Strategies & Recommendations

Two alternate catalytic diagrams were created for the Hampton Avenue/Appleton Avenue/84th Street/Grantosa Drive node. The following bullet points describe the alternatives presented in Concept 1 (Figure 5-8) and Concept 2 (Figure 5-9).

### Concept 1

- Establish a significant gateway and pedestrian feature at the primary intersection, which could include a public plaza or farmers market, landscaping, pedestrian amenities, and traffic calming measures to increase pedestrian safety
- When there are opportunities for infill redevelopment or business expansion, encourage buildings that reinforce the street edge by locating parking behind or alongside the building

- Incorporate green focal points, which may include community gardens, with pedestrian trails throughout the study area, which adds value to the neighborhood and encourages sustainable stormwater management
- Reinvest in the existing “medium box” as a multi-tenant reuse with compatible commercial and institutional users. For example, the building could be retrofitted to include a neighborhood library paired with companion commercial uses like a small coffee shop.
- Define “green” parking courtyards with buildings that hold the street edge; establish internal parking and vehicular circulation
- Incorporate pedestrian and bicycle linkages between the neighborhoods and shopping/service areas, where possible

Figure 5-8. Hampton Avenue / Appleton Avenue / 84th Street / Grantosa Drive Gateway Area Concept 1



- Improve vehicular circulation by limiting curb cuts and consolidating access drives

**Concept 2**

The alternative presented in Concept 2 builds upon the objectives in Concept 1. In addition to supporting the bullets outlined above, Concept 2 proposes:

- Dissolve the existing “medium box” and redevelop with mixed-use, residential units, and green space
- Surround the public plaza space with mixed-use buildings to create a courtyard condition
- Incorporate new, high-quality residential options into redevelopment efforts, which include green space and links into the adjacent neighborhoods

**Responsible Parties**

- Developer(s)
- Property Owner(s)
- Department of City Development
- Redevelopment Authority of the City of Milwaukee
- Department of Public Works

**Timing**

This option should begin with a discussion between the City, developers, and affected property owners to analyze and determine potential opportunities that are a result of the current economic climate and local market conditions.

Figure 5-9. Hampton Avenue / Appleton Avenue / 84th Street / Grantosa Drive Gateway Area Concept 2





## **LOCAL BUSINESS AND NEIGHBORHOOD SHOPPING DISTRICTS**

Within the West Side area, there are several local business and neighborhood shopping districts, each with a distinct identity and charm. Throughout the planning process, residents and business owners have expressed their support for these business districts, as well as their belief that vibrant local business and neighborhood shopping districts are an important part of their neighborhood and significantly improve their quality of life.

The following catalytic opportunities and tools aim to enhance these unique local business and neighborhood shopping districts and support their continued growth.

### **LOCAL BUSINESS AND NEIGHBORHOOD SHOPPING DISTRICTS**

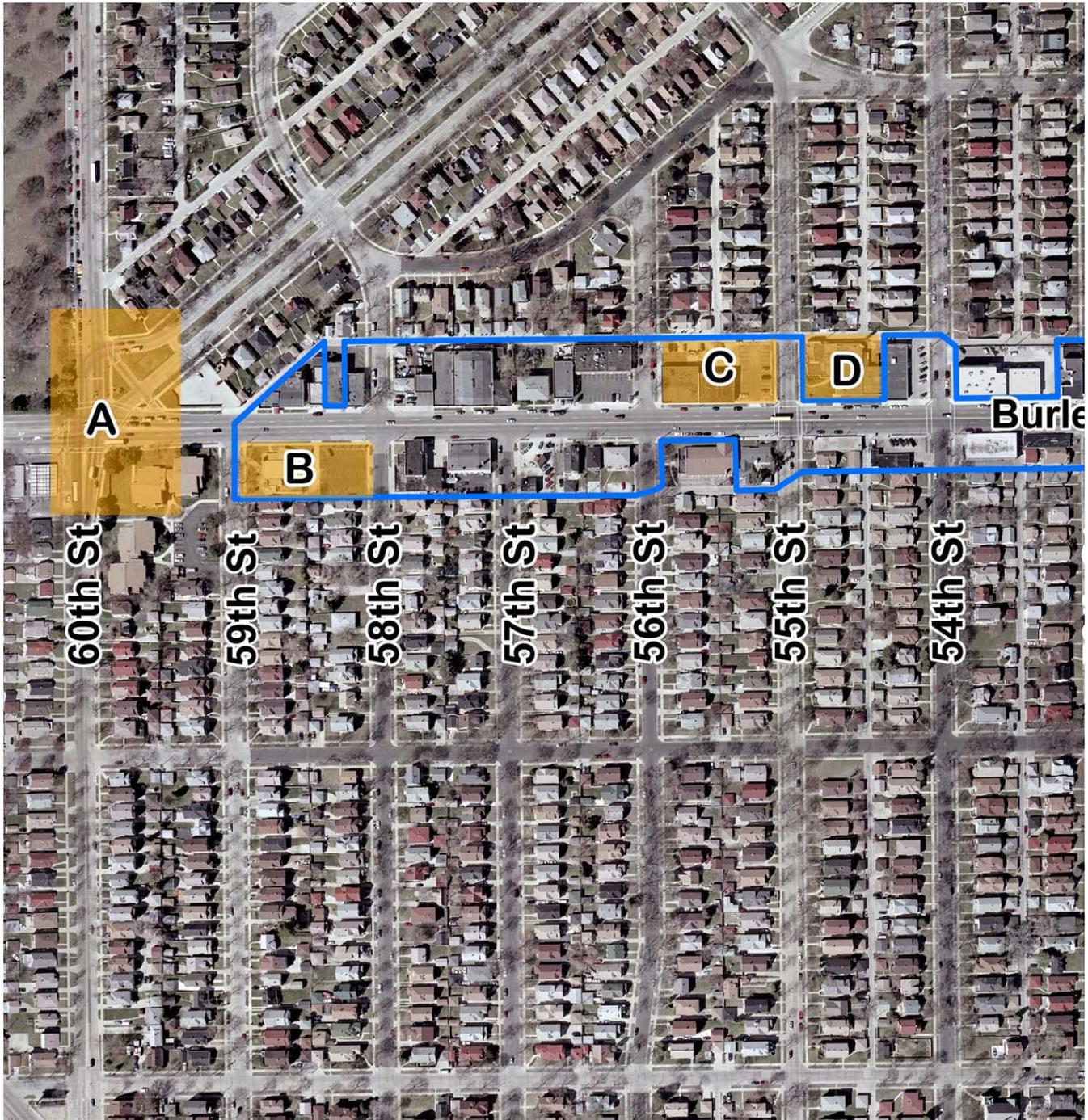
#### **REDEVELOPMENT OPPORTUNITIES**

The following maps are intended to further describe redevelopment opportunities within a limited number of neighborhood-oriented commercial districts. The identified assets and opportunities consist of the following: (1) anchors and landmarks; (2) destination retail; (3) historic structures; (4) gateway areas or locations; (5) buildings where rehab and renovation will improve the marketability of the district; (6) public places where improvements are needed; (7) building maintenance or code items that need to be addressed; (8) opportunities for public art, green space, or streetcape; (9) areas that will further specific business district goals and advance marketing efforts; and (10) sites that are susceptible to change or have experienced a recent change in ownership.

Redevelopment opportunity maps and descriptions are included for the following neighborhood-oriented shopping districts:

- Burleigh Street, BID #27
- Center Street Marketplace, BID #39
- Midtown Center
- North Avenue “Uptown Crossing,” BID #16
- Vliet Street Commercial District

# BURLEIGH STREET BID #27

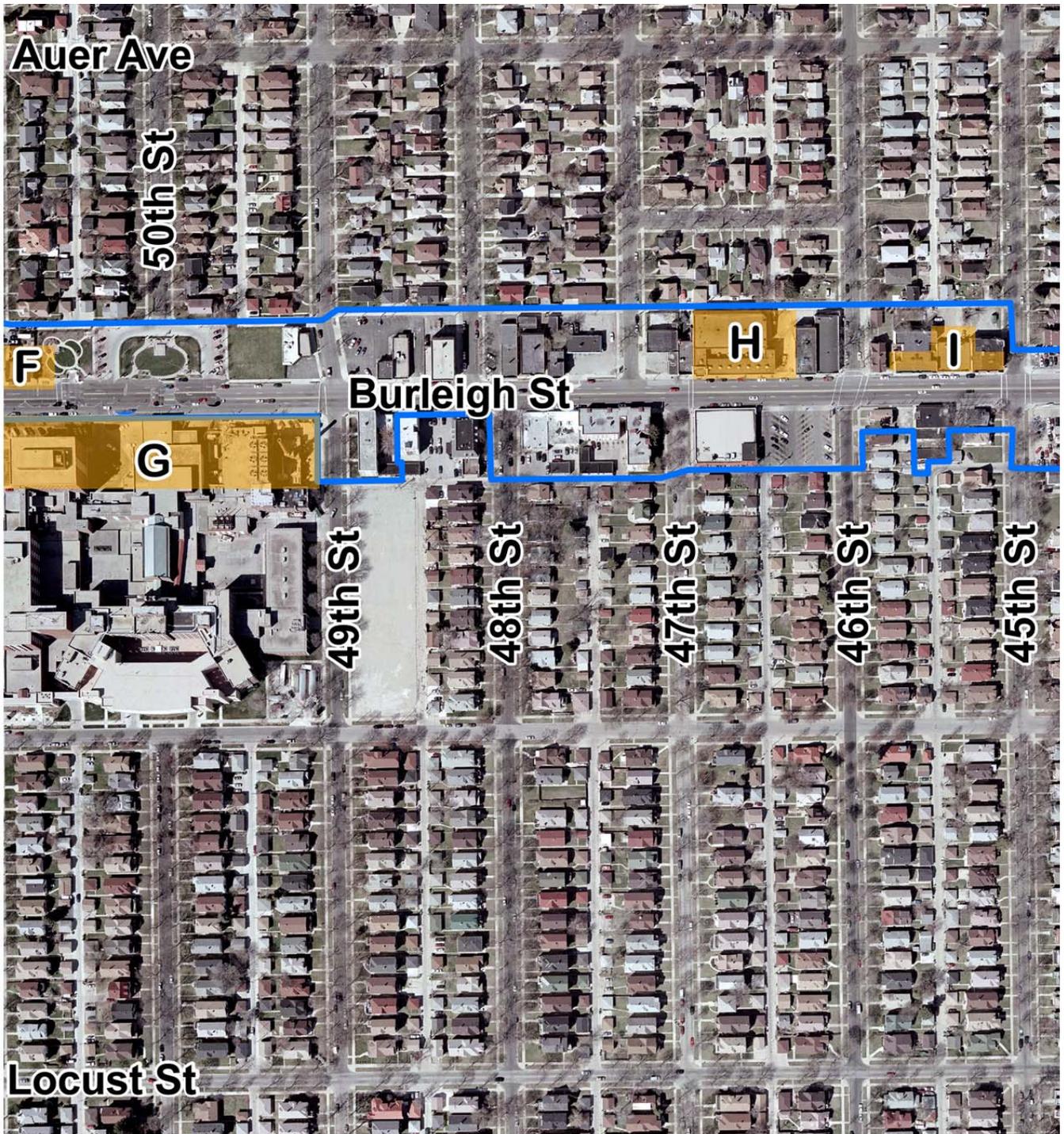


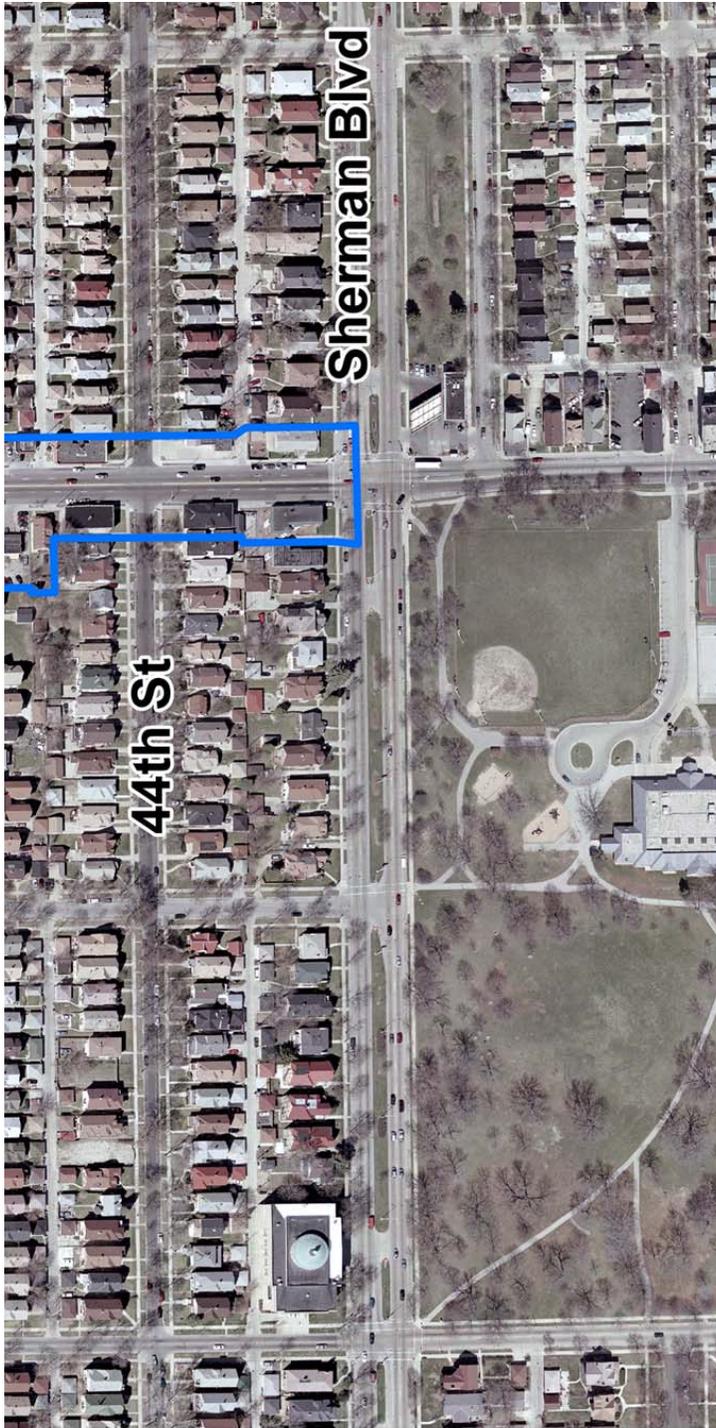
Notes:

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| <ul style="list-style-type: none"> <li>1: NE Corner 45th/Burleigh<br/>Car Champs, needs landscape and street edge improvements</li> <li>2: SE Corner 45th/Burleigh<br/>TNT Auto Repair, needs landscape and street edge improvements</li> <li>3: Southside of Burleigh 45th to 46th Street<br/>Isolated residences may be converted to commercial use</li> <li>4: SE Corner 48th/Burleigh<br/>Kosher Market is destination retail for Sherman Park Orthodox Jewish community, needs more street-friendly appearance</li> </ul> | <ul style="list-style-type: none"> <li>5: South side of Burleigh 46th to 47th Street<br/>Lena's Food Store Sherman Park is destination retail, needs landscape improvements to parking lot</li> <li>6: BID plans to fund facade improvements for architecturally significant buildings</li> </ul> |
|--|---|



- A: Intersection of 60th/Roosevelt/Burleigh  
Should be rationalized and improved for safety and appearance
- B: SE Corner of 59th and Burleigh St  
Former funeral home could become a family restaurant or other viable reuse
- C: North side of Burleigh St between 55th and 56th  
Derelict building should be acquired, rehabbed, and storefront restored
- D: NE Corner of 55th and Burleigh St  
Building should be acquired and rehabbed
- E: NW Corner 51st and Burleigh St  
Former BP gas station redevelopment site (adjacent to synagogue)
- F: NE Corner of 51st and Burleigh St  
Vacant property next to Pacific Orient needs retail tenant, opportunity to do outdoor cafe overlooking St Joe's park

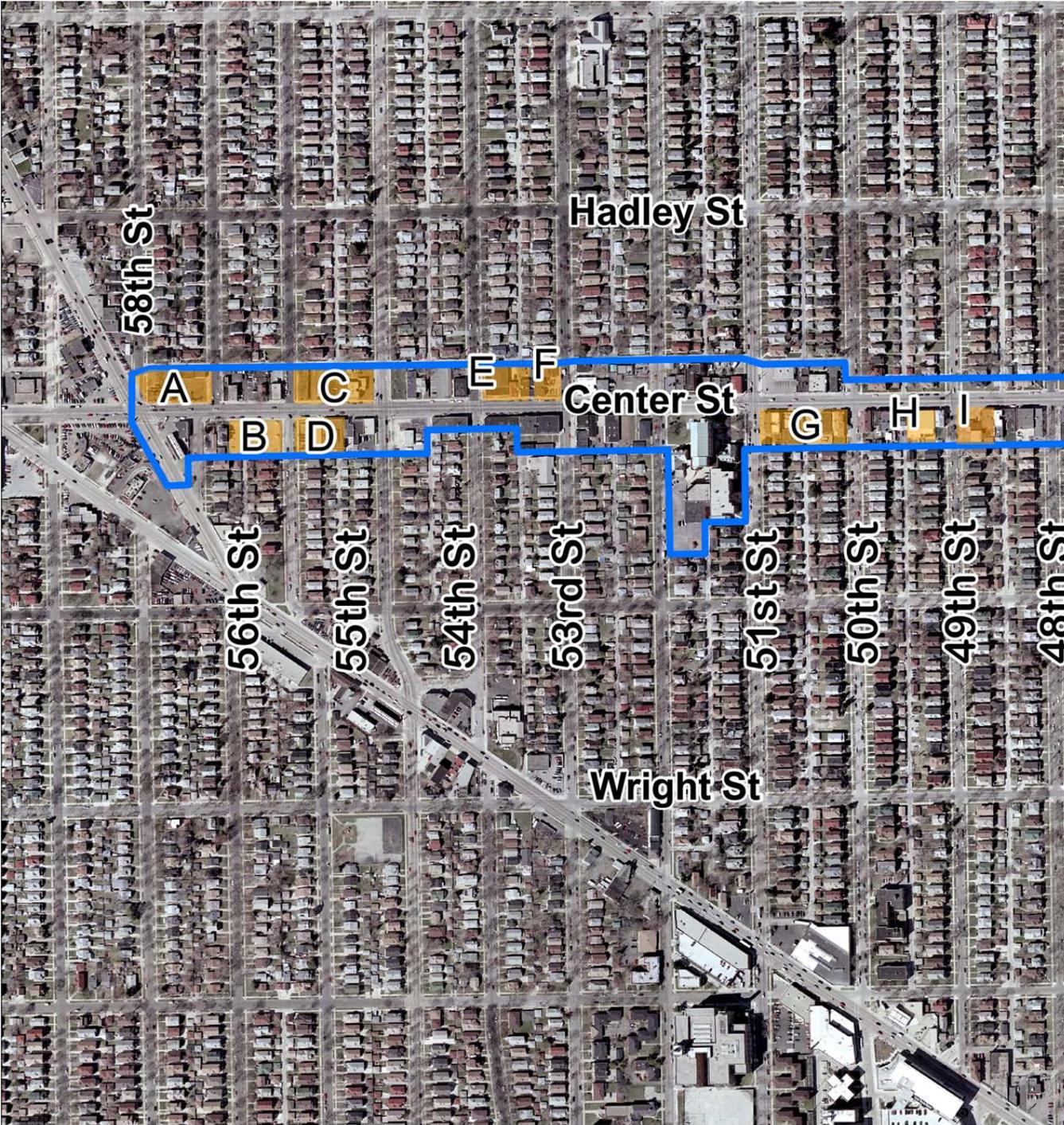


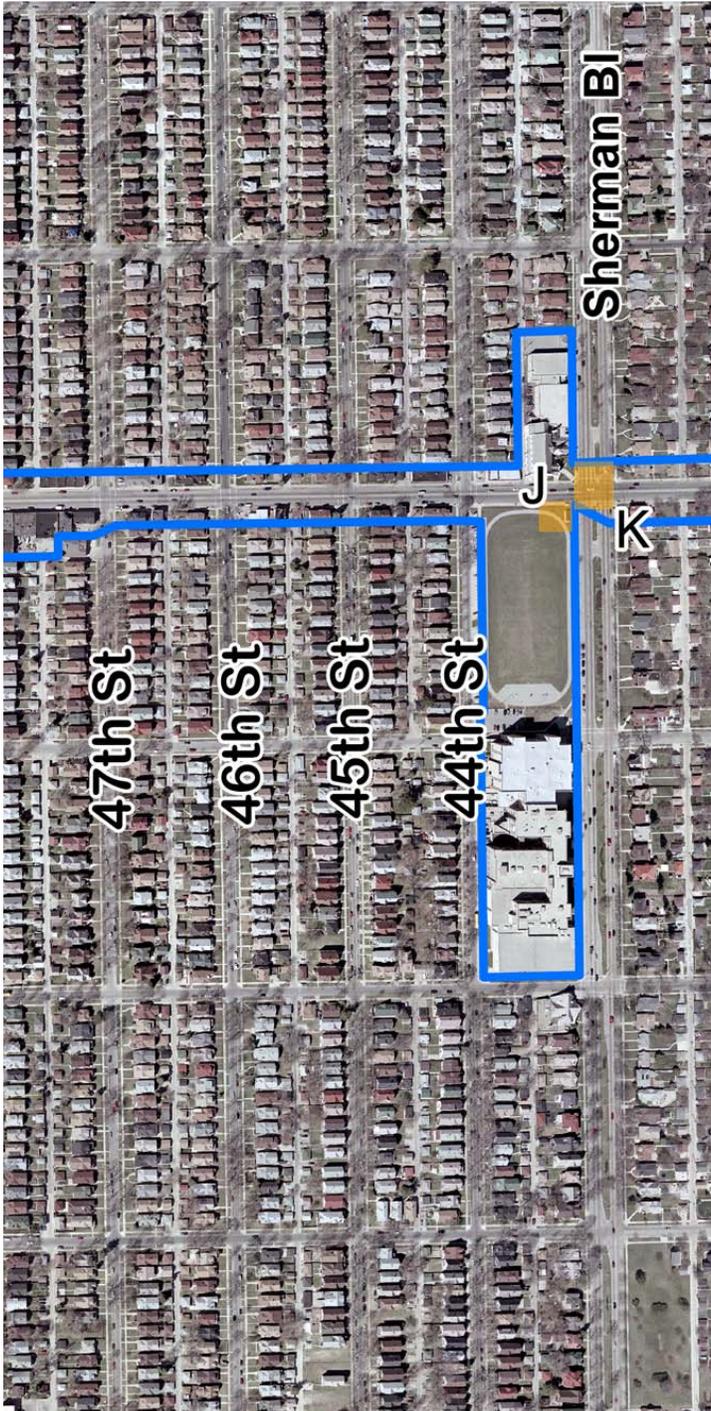


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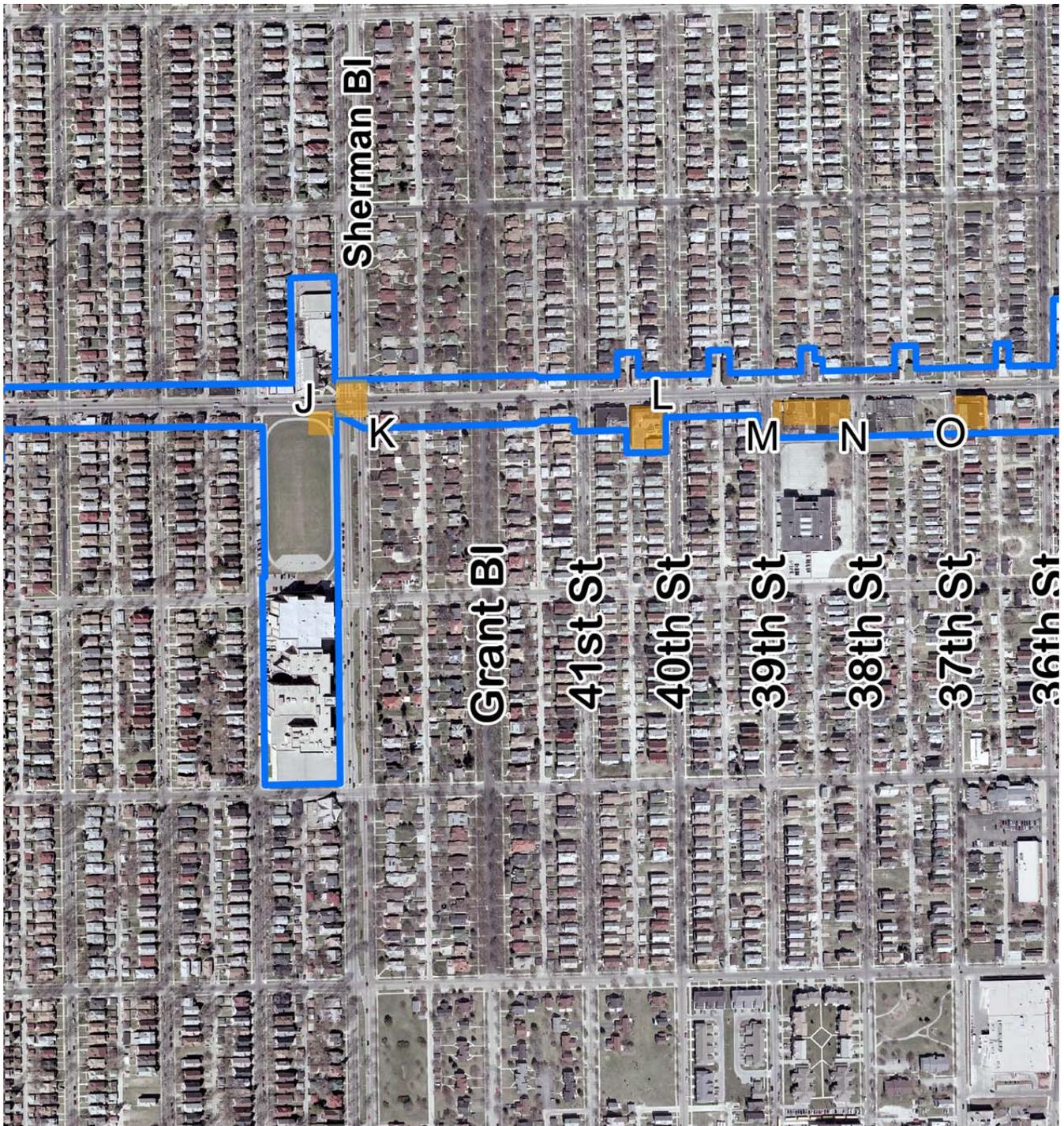
- G: South side of Burleigh St between 49th and 51st  
St Joseph's Hospital (Wheaton Franciscan Health Care) is major anchor
- H: North side of Burleigh St between 46th and 47th  
Sherman Theater Building (landmark building)  
needs anchor tenant
- I: North side of Burleigh St between 45th and 46th  
Building in mid block presents redevelopment  
opportunity

# CENTER STREET MARKETPLACE BID #39



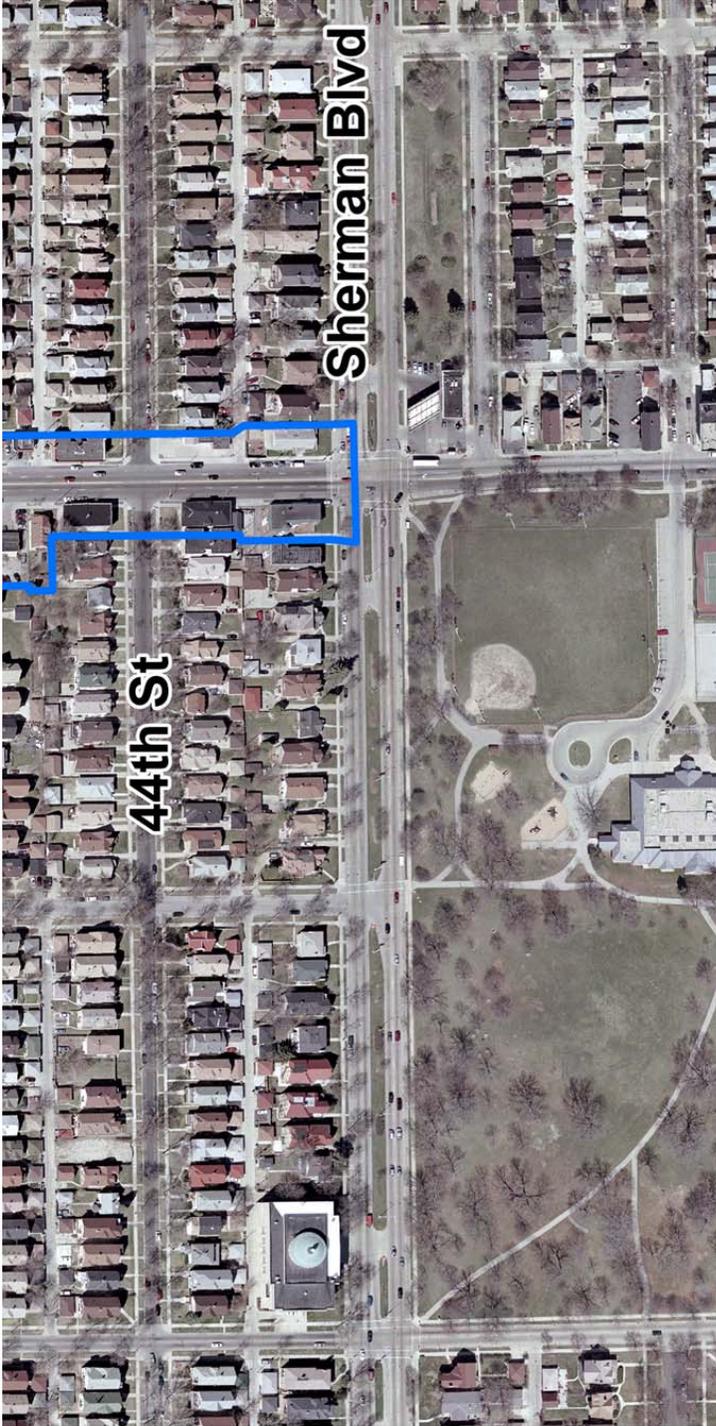


- A: NE Corner of 58th and Center St  
Law Office, anchor property
- B: SW Corner of 56th and Center St  
Redevelopment opportunity, Post Office lot
- C: North side Center St between 55th and 56th St  
Model block with street level retail and apartments above
- D: SE Corner of 56th and Center St  
Redevelopment opportunity, needs improvement in appearance
- E: NW Corner of 53rd and Center St  
Building up for auction, redevelopment opportunity
- F: North side Center St between 53rd and 54th St  
Building up for auction, redevelopment opportunity
- G: South side Center St between 50th and 51st St  
5048 W Center is redevelopment opportunity
- H: SW Corner of Center and 49th  
Vacant corner store is a redevelopment opportunity
- I: SE Corner of Center and 49th St  
Redevelop corner as pedestrian oriented commercial use
- J: SW Corner of Sherman Blvd and Center St  
Washington High School needs identity features, landscaping
- K: Intersection of Sherman Blvd and Center St  
Needs to read more as a gateway intersection



- notes:
- 1: SE Corner 55th/Center  
Large lot used for outdoor storage of antennas, needs screening (special use that does not contribute to commercial district)
  - 2: SE Corner 38th/Center  
Possible candidate for facade grant
  - 3: Blocks between Sherman Blvd and 41st  
Maintain as residential near 41st and Center St
  - 4: South side Center, 53rd to 54th Street  
Facade grants would improve appearance

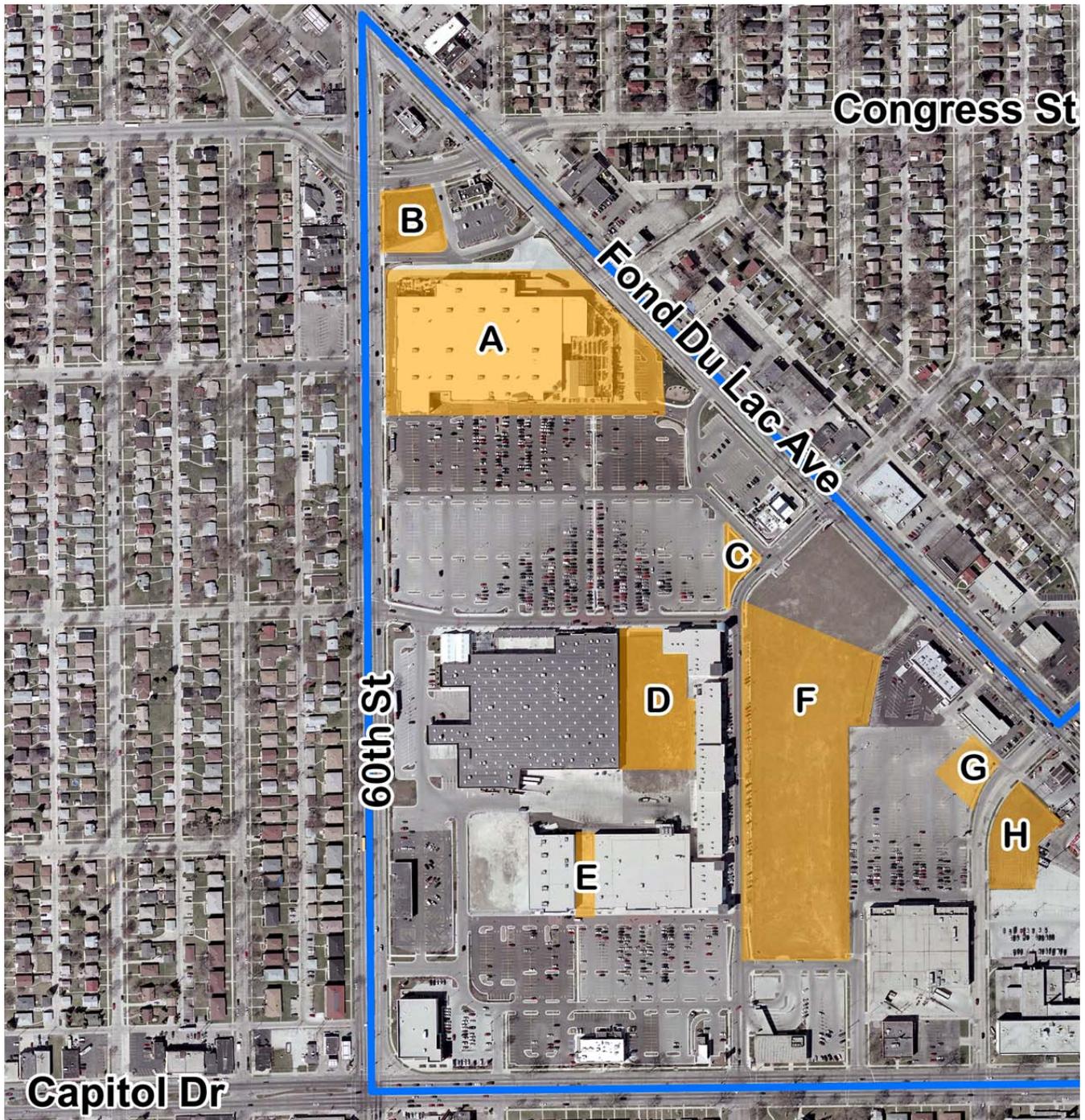
- 5: SE Corner 53rd/Center  
Vacant buildings in need of facade work, signage
- 6: NW Corner 52nd/Center  
Mr. P's Tires, needs facade/site improvements
- 7: NE Corner 52nd/Center  
Barber shop, needs to clean up facade, remove weeds
- 8: SW Corner 36th/Center  
Boarded up building in need of facade repair
- 9: NE Corner 51st/Center  
Parking lot needs landscaping, fencing



continued...

- G: South side of Burleigh St between 49th and 51st  
St Joseph's Hospital (Wheaton Franciscan Health Care) is major anchor
- H: North side of Burleigh St between 46th and 47th  
Sherman Theater Building (landmark building)  
needs anchor tenant
- I: North side of Burleigh St between 45th and 46th  
Building in mid block presents redevelopment  
opportunity

# MIDTOWN CENTER



- notes: 1: Improve corridors surrounding Midtown Center
- 2: Redevelop interior streets
- 3: Improve connections between Midtown and peripheral uses.
- 4: Add landscaping and streetscaping



- A: Former Lowe's Store - Redevelop as commercial "big box" compatible with Midtown Center uses
- B: Out lot available for lease
- C: Green area would provide a gateway opportunity for sculpture or landscaping
- D: Potential expansion site for Wal-Mart
- E: Potential infill site/expansion
- F: Vacant land and retail space is an opportunity for new businesses
- G: Out lot Possibility
- H: Vacant parking lot - re-use as infill development
- I: Infill site
- J: Rehabilitation near completion
- K: Gateway opportunity

# NORTH AVENUE “UPTOWN CROSSING” BID #16



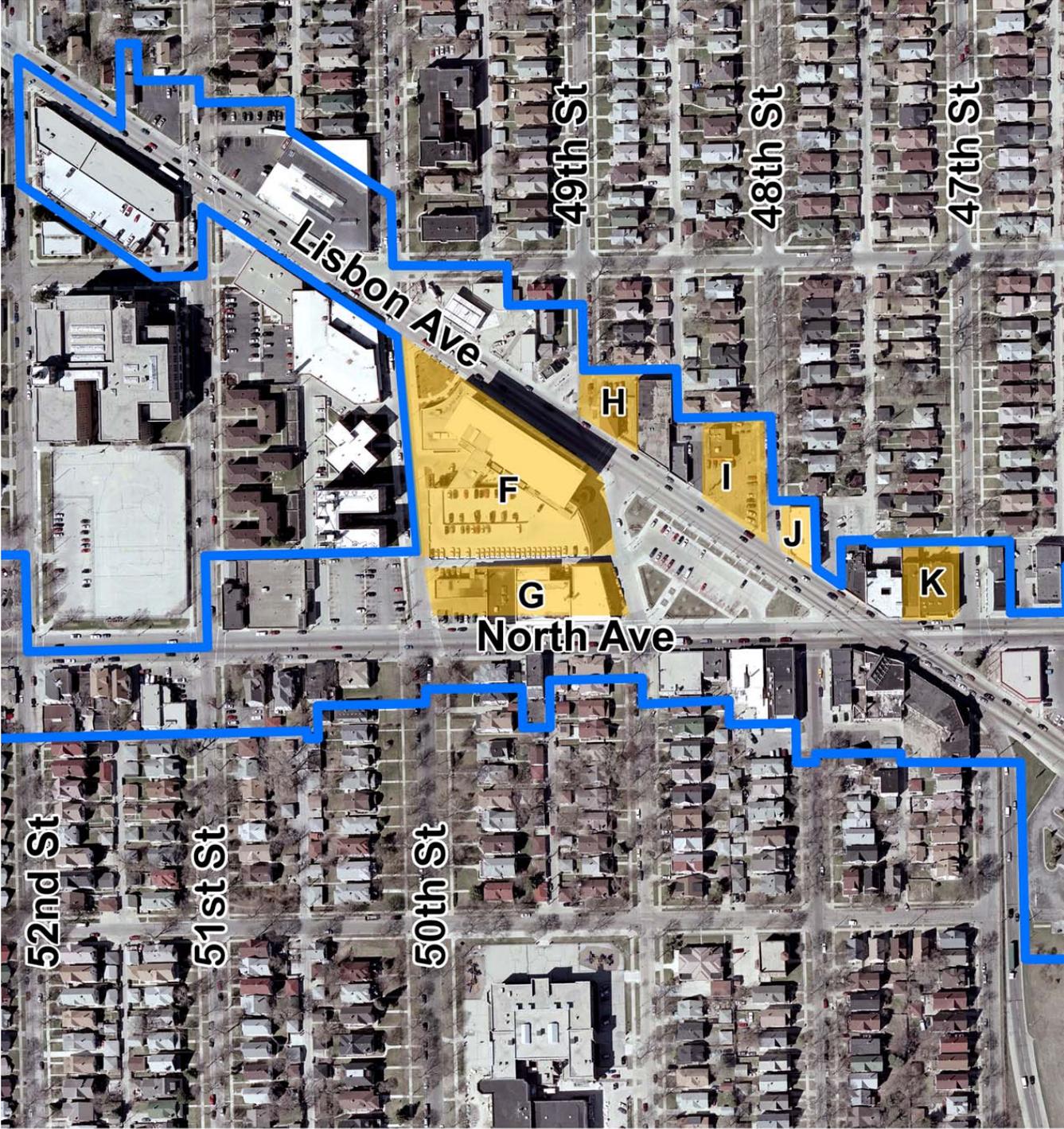
Notes:

1. Updown triangle at Lisbon/North/49th  
Landscaped public parking lot with public art at point of triangle (Lisbon/North/48th) used to host public events, outdoor markets, etc.
2. North side of 51st/Lisbon  
Lisbon Storm & Screen, anchor tenant but street edge is harsh, needs greening and streetscape treatment
3. SW corner of 59th/North  
Used car lot, needs more attractive street frontage



- A: Gateway Intersection at 60th and North Ave  
Needs further enhancement and Identity features
- B: NE Corner of 60th and North Ave  
Gaslight Building has potential for adaptive reuse
- C: SE Corner of 59th and North Ave  
Tire center needs landscaping, screening, and a more positive appearance
- D: NW Corner of 55th and North Ave  
Mecca Store is vacant and available for new tenants
- E: South side between 55th & 56th along North Ave  
Several storefronts are vacant and available for new tenants

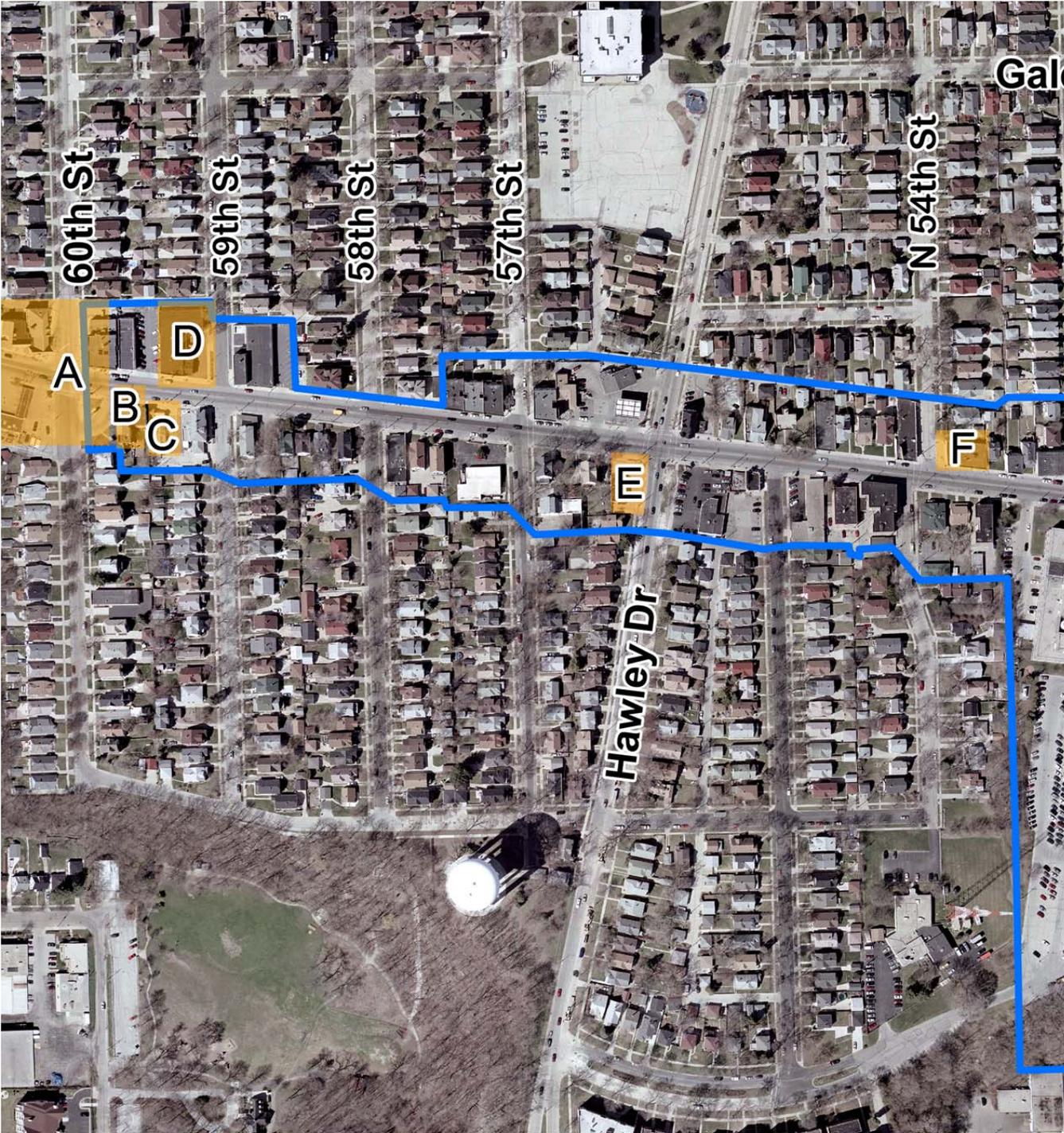
- 4. NW corner of 53rd/North  
Former funeral home, now successful charter school
- 5. Southside of 52nd/North  
Nicely renovaed cluster of businesses (critical mass)
- 6. SW side of 48th/Lisbon/North Triangle  
Need for traffic calming to help viability of business at "Hwy 41 on ramp"
- 7. North side of 51st/North  
MPS French Immersion School, opportunity for landscape improvements and benches along North Ave

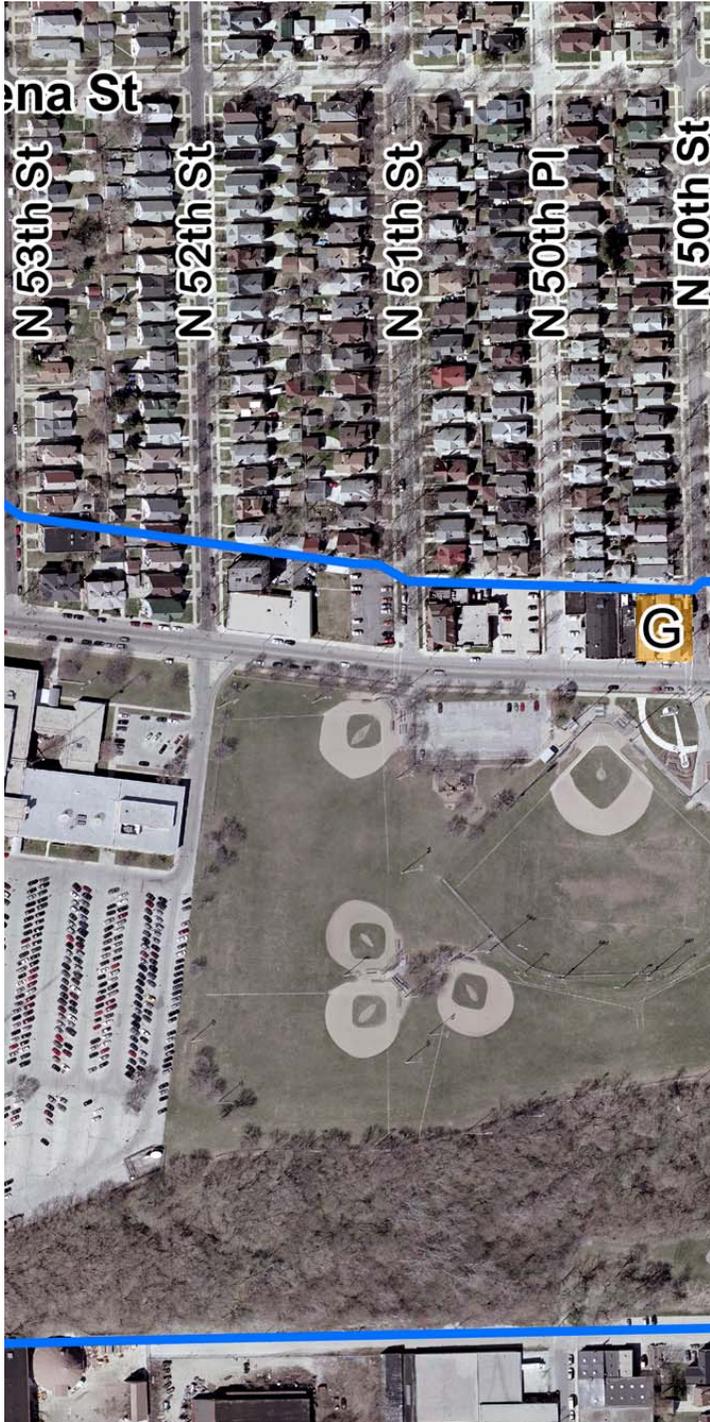




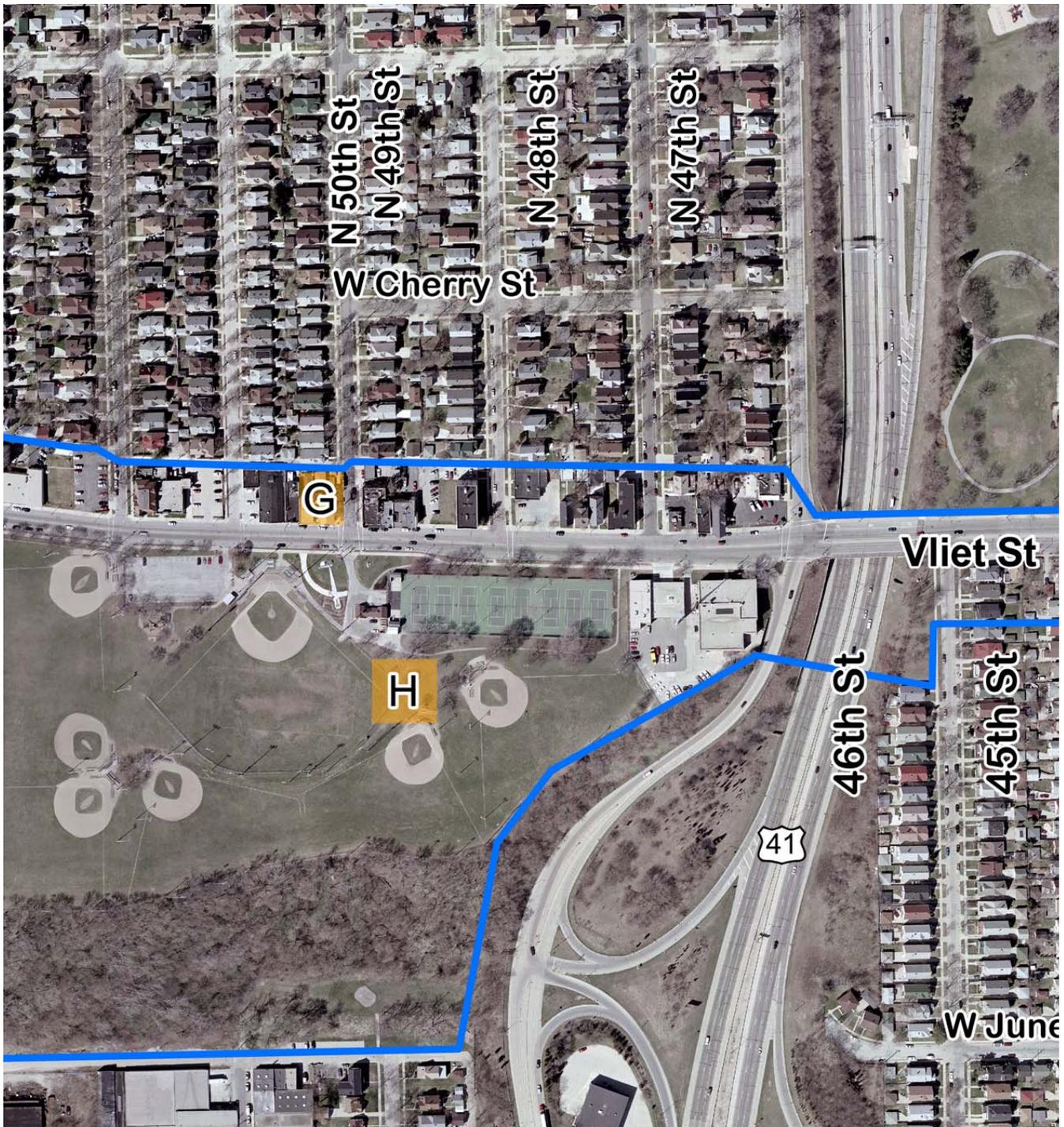
- F: SW Corner of 49th and Lisbon Ave  
Third District Police Station/Data Communications Center serves as non-retail anchor for district
- G: NW Corner of 49th and North Ave  
Successful grocery (Viet Hoa) has potential for increasing market -- needs facade improvement, expansion of store could include a cultural center to serve Hmong population in area
- H: NE side of 48th/Lisbon Ave/North Ave Triangle  
Two historic buildings with potential for reuse
- I: NW Corner of 48th and Lisbon  
Building and parking lot of (Judy's) in need of improved street edge
- J: North side of Lisbon Ave/ North Ave intersection  
Vacant Lots (tax delinquent building) are a redevelopment opportunity
- K: NW Corner of 47th and North Ave  
Problem properties, opportunity to cluster rehab and infill redevelopment
- L: NW Corner of 45th and North Ave  
Potential for new Walgreen's prototype at same location
- M: North side of 44th to 45th on North Ave  
Vacant properties in need of rehab, tenants (vacant lot NE Corner of 45th/North is a redevelopment opportunity)
- N: SW Corner of 45th and North Ave  
New Y-Mart, redevelopment or rehab opportunity
- O: SE Corner of 45th and North Ave  
Convenience store/grocery in need of facade improvements
- P: Gateway intersection at Sherman Blvd / North Ave  
Needs identity features, landscaping
- Q: Vacant lot north side of Lisbon is a redevelopment opportunity
- R: Vacant lot north side of Lisbon is a redevelopment opportunity

# VLIET STREET COMMERCIAL DISTRICT

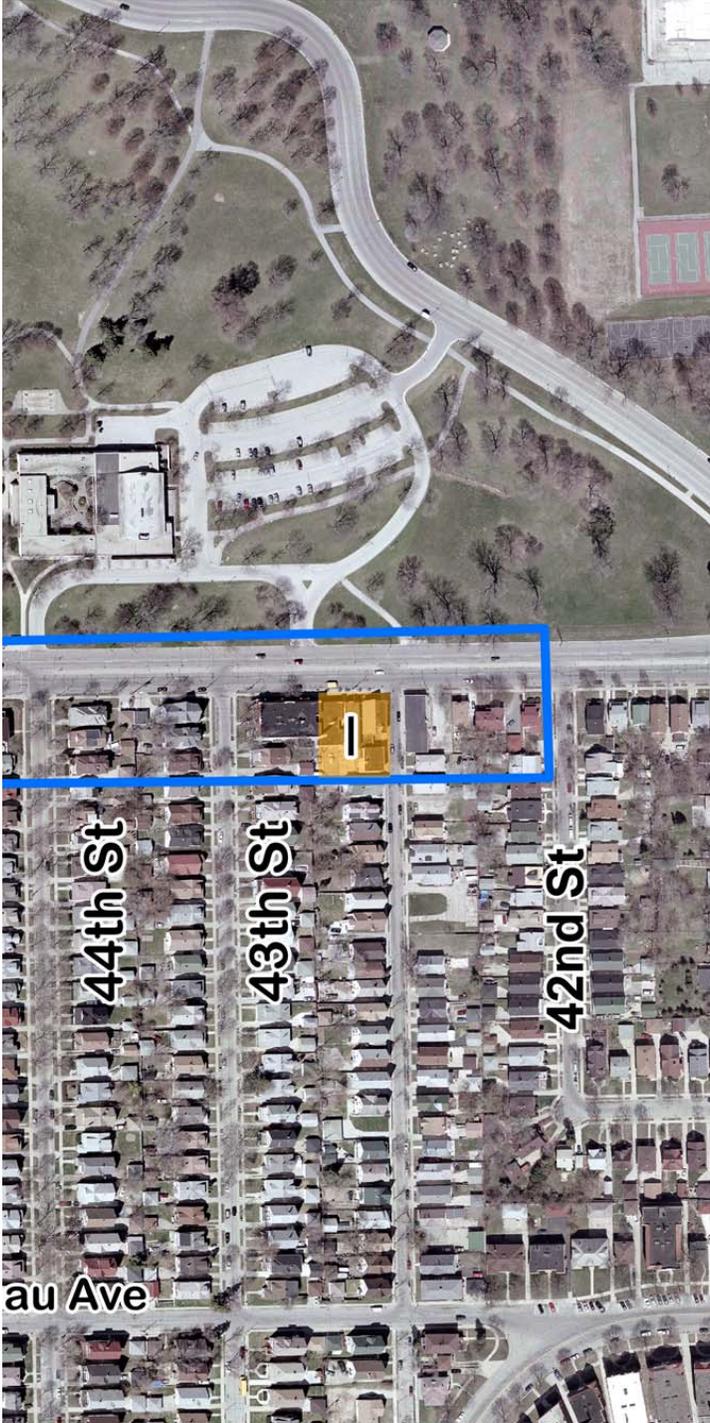




- A: 60th/Vliet  
Gateway intersection needs identity features and landscaping
- B: 5921 W Vliet  
Meritage, destination restaurant
- C: 5911 W Vliet  
Westside Garden Center, nursery/greenhouse is an anchor for district
- D: 5906 W Vliet  
Times Cinema, one of district's original anchors, family-oriented entertainment
- E: SW Corner of Hawley/Vliet  
Property needs parking lot and facade improvements
- F: NE Corner of 54th/Vliet  
Redevelopment opportunity, former bakery
- G: 5000 W Vliet  
Elements East, upscale Asian artifacts and furniture (destination retail)



- notes:
- |  |  |
|--|--|
| <p>1: SE Corner 59th/Vliet<br/>Building needs facade improvements</p> <p>2: SW Corner 57th/Vliet<br/>Building needs facade improvements</p> <p>3: SE Corner of 43rd/Vliet<br/>Nut Company building needs better facade, more street-friendly appearance</p> <p>4: NW Corner of 53rd/Vliet<br/>Gietl Sign Company, concrete block wall needs facade improvement, opportunity for landscaping at street edge</p> | <p>5: 5308 W Vliet<br/>House could be commercial redevelopment opportunity</p> <p>6: South side of Vliet between 52nd and 50th<br/>Wick Field needs improvements/amenities, also needs better signage in regard to availability of MPS Central Administration parking lot</p> <p>7: Vliet Street Commons in Wick Field - remove unnecessary electrical poles and junction boxes</p> <p>8: Extend harp lights to east end of district (42nd to 47th St)</p> |
|--|--|



G: 5000 W Vliet

Elements East, upscale Asian artifacts and furniture (destination retail)

H: Wick Field

Make improvements to Wick Field (fencing, parking lot, landscaping, facilities, and lighting)

I: SW Corner of 43rd/Vliet

Model cluster of renovated buildings, consider that block a gateway, green the median, improve surrounding properties to same high level.

**LOCAL BUSINESS AND NEIGHBORHOOD SHOPPING DISTRICTS**

**ILLUSTRATED DESIGN MANUAL**

The Illustrated Design Manual (located in Appendix 2) is intended to provide a range of stakeholders - including BID boards, property owners, business owners, developers, designers, and the City - with a set of evaluation parameters and illustrated examples for a range of enhancement concepts - from new signage to facade improvements - by which infill development proposals can be created and evaluated.

The following are the important objectives of the guidelines:

- To aid the successful implementation of the City's general urban design standards
- To ensure that high quality development creates a vibrant, diverse, clean, and safe destination with sustainable economic vitality
- To create meaningful and active public places and streets through effective design and proper placement of building entrances and landscape features
- To ensure that building and site designs create a safe, attractive, and interactive street for pedestrians, cyclists, and motorists
- To ensure that the character of future development complements existing uses and the scale of neighboring development
- To allow designers and developers reasonable flexibility in the creation of specific designs to meet current and future market and economic realities
- To create and maintain optimal economic and social value as these areas develop and redevelop over time
- To see that vehicular access and parking needs are effectively addressed while any negative impacts on the urban form and pedestrian experience are minimized

There are two basic components to the Illustrated Design Manual. The first is a Design Review Checklist that provides a series of topics and subsequent questions intended to guide the review process and call attention to design features that must be considered. This portion of the manual is an important tool when reviewing proposed building improvements or a development proposal, but is also useful for property owners to use during the design process.

The second component incorporates a wide variety of photographic examples into a series of tables. Organized by feature categories (architectural design, site design, landscape and streetscape, and signage), the table shows users which examples are substantially compliant or partially compliant. As with the Design Review Checklist, the tables are meant to provide visual examples to ensure that significant property improvements and development projects add value to the neighborhood, while respecting the context of the established shopping district.

As a supplement to the West Side Area Plan, the Design Manual provides the basic framework and guidance needed to begin a structured review process. However, it is intended to serve as a foundation and not a finished product, as each local business and neighborhood shopping district is unique. It is recommended that either the City and/or each individual BID/Business Association take ownership of the manual and tailor the questions to suit their distinct identities. Further, the manual could also be tailored for the many aging, commercial corridors on the West Side.

## NEIGHBORHOOD IMPROVEMENT DEMONSTRATION PROJECT

The combined Enderis Park “Neighborhood Image Plan” and Lenox Heights “Neighborhood Improvement Plan” (see Appendix 3 for the full text of both Plans) will be the West Side’s first Neighborhood Improvement Demonstration Project. As such, it demonstrates a number of ways to build upon the assets and strengths of an established urban neighborhood. The Neighborhood Improvement Demonstration Project supports the vision of creating a sustainable neighborhood with a clear identity within the West Side community. In addition to establishing an action plan for Enderis Park and Lenox Heights, the Demonstration Project serves as a model for other West Side Neighborhoods.



Above: An example of existing, standard City signage within the Enderis Park neighborhood. The signage does not give identity to the neighborhood and appears cluttered. Below: A gateway monument sign for the Enderis Park neighborhood, which serves as a gateway into Milwaukee from Wauwatosa as well as the neighborhood.



The Neighborhood Improvement Demonstration Project (NIDP) is centered around the following organizing principles:

- Enhance and maximize assets and strengths of the neighborhood, particularly parks and green space, but not limited to those.
- Market and promote the neighborhood to attract visitors or potential homebuyers and to endear the neighborhood to longtime residents.
- Create an internal neighborhood organization that takes advantage of residents’ talents and willingness to volunteer.
- Organize activities that people enjoy such as youth activities, holiday celebrations, chess tournaments, ice cream socials or concerts in the park.
- Provide a social network for residents that strengthens communications, and ensures that problems or challenges are dealt with in a timely and inclusive way.
- Make stronger user-friendly walkable connections between residential blocks and local business and neighborhood shopping district(s). Work to improve the mutually beneficial relationship between stores that serve the needs of neighborhood residents, and residents who provide a stable “walking distance” market for goods and services.
- Find ways to enhance neighborhood identity and “personalize” the standard kit of parts that make up the City’s basic infrastructure, such as streets, street furniture, public right-of way improvements, landscaping, streetscape elements, sidewalks, intersection paving and crosswalks, bike paths/lanes, traffic signs, light fixtures, utility boxes (public and privately owned), public art and other identity features.

## MAJOR & ONGOING INITIATIVES

Within the West Side, as well as every other part of the City, a range of agencies, neighborhoods, and other organizations are involved in various planning processes. While these initiatives are independent efforts and not directly a result of the larger area planning process, they play an important part in the larger picture. The intent of the West Side Area Plan is to support these diverse efforts, serving as a larger umbrella policy plan at the city government level. In that spirit, the following two initiatives are major undertakings in the area. The Master Plan for the Revitalization of Westlawn is currently in the planning phase with implementation anticipated in the near future. The visioning and strategic planning effort for Mosaic on Burleigh was completed in 2005 and is in an ongoing implementation phase.

## MAJOR & ONGOING INITIATIVES

### MASTER PLAN FOR THE REVITALIZATION OF WESTLAWN

Built on a 75 acre campus, Westlawn is Wisconsin's largest multi-family development with 726 housing units, 709 of which are occupied. The development is located south of Silver Spring Drive, between 60th and 68th Streets, and includes a school, neighborhood center, and many other resources that are utilized on a daily basis by residents and surrounding neighbors. However, the existing site layout does not connect to the street grid, isolating residents from the adjacent neighborhoods and limiting potential for commercial development along Silver Spring Drive.

The Housing Authority of the City of Milwaukee (HACM) is currently in the process of working with consultants (Torti Gallas and Partners, Kindness Architecture + Planning) and area residents to create a master plan for the site. The goals guiding the planning process and, ultimately, the

Figure 5-10. Illustrated Design Concept for Westlawn, working draft (Source: Torti Gallas)



redevelopment of the Westlawn neighborhood are listed below and generally illustrated in Figure 5-10:

- Reintegrate the neighborhood into the surrounding street grid and broader community
- Establish a mixed-use neighborhood, including retail and service uses along Silver Spring Drive
- Create a mixed-income neighborhood with a variety of housing choices and opportunities for community interaction (e.g. front porches, shared open spaces, community center)

*“Modern urban design emphasizes connecting people through the use of elements such as front porches. There’s more consideration to how people are engaging each other and how they are engaging the street. There’s also more attention to safety or “eyes on the street.” ”*

**Westlawn Resident Association**

It is anticipated the Master Plan for the Revitalization of Westlawn will be completed by the end of 2009. Redevelopment of the HACM site is expected to follow in 2010.

**MAJOR & ONGOING INITIATIVES**

**MOSAIC ON BURLEIGH - VISIONING & STRATEGIC PLANNING FOR BURLEIGH MAIN STREET DISTRICT AND BID #27**

**Background**

The Burleigh Main Street Visioning and Strategic Planning Report, drafted by Downtown Professionals Network (DPN), was introduced in July 2005 as a way to kick-off a Burleigh Main Street program. The Burleigh Main Street District “spans Burleigh Street from Sherman Boulevard



west to 60th Street. The district developed as a commercial center serving the needs of residents in the neighborhoods immediately surrounding the district and, historically, from throughout the Sherman Park neighborhood.”



The DPN team held a three-day Vision Design session, and all activities, findings, and direction based on public input are summarized in the report. Concepts and strategies that were taken that were taken to the next level of design and development included streetscape enhancements, image enhancement, recognition of current area character, and the creation of “activity generators.”

The framework also encompassed the project area, built environment, streetscape, business mix and investment climate.

**Recommendations & Guiding Principles**

The widely varied character of “development spanning the Burleigh Main Street district corridor, along Burleigh Street from Sherman Boulevard west to 60th Street, poses both challenges to, and opportunities for, achieving the community’s vision for the district as a safe and pedestrian-friendly neighborhood commercial center for Sherman Park.”

Through a comprehensive public participation process, which included stakeholders from diverse backgrounds and varied interests, the Visioning Team was able to gain consensus around a series of basic principles and common causes. This provided a foundation for the Burleigh Main Street District “Guiding Principles for Design & Development,” as follows:

1. Induce traffic to move at neighborhood appropriate speeds (traffic calming).
2. Encourage pedestrian activity along and across Burleigh Street.
3. Recognize the diverse character of development along the length of Burleigh Street.
4. Focus on the creation of “Third Places” (places for social interaction) at key locations along the street.
5. Renovate buildings in a way that respects the existing architecture. Develop new buildings that reflect present-day architecture but are compatible with its surroundings.
6. Foster a high-quality, vibrant business mix that is representative of, and that caters to, the diverse tastes and needs of area residents and employees.

These principles played a critical role in developing a series of design concepts (Figure 5-11). The redevelopment and revitalization design concepts focus on “activity zones” that are created by “diverse development patterns found along Burleigh Street.” Enhancement concepts are also introduced. The concepts address the following areas:

- East End - Sherman Boulevard to 49th Street
- Central Burleigh - 49th Street to 54th Street
- West End - 54th Street to 60th Street

### **Implementation Strategies**

According to the report, “The outcome of the process [tailored to engage Burleigh Main Street residents, business persons, stakeholders, and civic leaders in a series of community-oriented visioning and strategic planning activities] provides direction for short- and long-term initiatives and a framework for the Burleigh Main Street program’s first year work plan outlining strategic actions and projects that are highly relevant to Burleigh Main Street stakeholders’ vision for the district.

The visioning and strategic planning process also helped generate consensus around short- and long-term goals and objectives, fostered a strong sense of ownership in local Main Street program plans and projects, and instilled a spirit of “teamwork” that is critical to building the capacity and sustaining the efforts of Burleigh Main Street.”

*Specialty shops are the neighborhood's trademark! Located here are European bakeries, Kosher delis, and stores packed with health foods, unique clothing, fine furniture, antiques, music and musical instruments, gardening supplies, and decorating/renovation materials. Walking along the commercial strips is a treat as storefronts introduce one pleasant surprise after another.*

Sherman Park Website

Implementation recommendations, including goals, objectives, and prioritized action steps, were outlined to correspond with the Main Street Program’s committee structure. This ensured that each recommendation had a responsible party for implementation within the existing organizational structure of the Mosaic on Burleigh.

To assist in implementing design concepts for the Mosaic on Burleigh Main Street Program (resulting from the visioning and strategic planning process)-Burleigh BID #27 has hired a consultant to do a strategic business development plan for the eastern end of the district. Its goal is to make that area a catalytic development node for the district.

The two major opportunities for improvement of the business climate on Burleigh are the renovation of the former Sherman Park Theater Building and spin-off development related to and near St. Joseph Hospital. The Sherman Park Theater (in the building named for it) is a local landmark. It was at one time a popular neighborhood cinema and an ornate theater that brought vaudeville productions to Sherman Park. It is currently under renovation and will reopen soon. St. Joseph Hospital reinforces what is already a strong local economy and significant consumer base in Sherman Park.



## Ongoing Efforts

Over the past few years, the Burleigh Main Street District and its associated committees have made the Milwaukee Main Street program a successful work in progress:

- The Promotion Committee has developed a series of strategies: tapping into consumer markets, hosting special events, providing a framework for retail events and activities, and consistently promoting image enhancement.
- The Design Committee is working to promote historic preservation and aesthetics, appearance and maintenance, improved parking conditions, and streetscape of the corridor.
- The Main Street Economic Restructuring Committee is in the process of strengthening the existing economic base by providing data collection and analysis, focusing on business retention and expansion, recruiting new businesses, and embracing new redevelopment opportunities.
- The Crime and Grime Committee is beginning to address crime and the perception of crime in the district. Its strategies include internal communication and partnerships, external communications and partnerships, tracking, monitoring, and reporting, and promoting positive public relations.

The success of the Burleigh Main Street District depends on the continued work of the committees, as described, and the help and cooperation of the businesses within the district. Building on this foundation, the district has the potential for becoming a leader for traditional neighborhood shopping districts elsewhere in the West Side Area and the City at large.

Success of the Burleigh Main Street Program also depends on organizing capacity and funding resources from BID #27. Principal activities from the BID #27 Operating Plan are as follows:

- Put out timely information on topics such as police patrol, garbage clean-up, marketing opportunities, and other topics of local concern, to members, retailers and others in the form of articles, fliers, e-mails and other appropriate methods.
- Create a business directory and encourage businesses to relocate to the district.
- Direct and/or collaborate with other appropriate agencies in the implementation of streetscape and other long-range plans approved by the board of directors.
- Encourage all commercial buildings to be maintained graffiti-free and financially support and coordinate the means to accomplish this goal.
- Encourage and financially support façade improvements to properties within the BID.
- Advise area businesses on safety and security measures and to serve as liaison



*Left: An existing vacant lot, adjacent to a former theater building and several storefronts. Below: A concept developed during the strategic planning effort to create an outdoor cafe, adjacent to the theater building*



with the security offices of area institutions and the Milwaukee Police Department.

- Liaison with owners of private and public property to encourage quality maintenance and management of said property.
- Provide staff assistance to property owners and developers who are engaged in property improvements and redevelopment actions.
- Market and work to achieve visual enhancement of the Commercial District.
- Engage in building exterior enhancement work for select properties within the BID.

